

Director of Planning Services  
Fylde Borough Council  
Town Hall  
Lytham St Annes  
Lancashire  
FY8 1LW

Tel 01772 533734  
Email david.watson@lancashire.gov.uk

Your ref 15/0547  
Our ref D5/15/0547/DRW3  
Date 16 May 2016

Dear Sir

**Planning Application No: 15/0547**

**Location: BROOK FARM, DOWBRIDGE, KIRKHAM, PRESTON, PR4 3RD**

**Description: OUTLINE PLANNING APPLICATION FOR DEMOLITION OF EXISTING BUILDINGS AND RESIDENTIAL DEVELOPMENT OF UP TO 170 DWELLINGS INCLUDING ASSOCIATED INFRASTRUCTURE (ACCESS APPLIED FOR WITH ALL OTHER MATTERS RESERVED)**

**(\*Note: these statutory comments relate to the application for 170 dwellings on this site (PA Ref. 15/0547), a separate response with similar highways conclusions has been produced in respect of the 95 Dwelling Application (PA Ref. 15/0827)**

I refer to the above application and would like to thank you for the opportunity to comment on the proposal.

Lancashire County Council (LCC) is responsible for providing and maintaining a safe and reliable local highway network in Fylde. LCC, as the local highway authority, embraces a one team approach, working closely with developers and the planning authority to deliver high quality, sustainable development. With this in mind the present and proposed traffic systems have been considered to highlight areas of concern that, potentially, could cause problems for the public, cyclists, public transport and motorists that will influence movement on the network.

LCC have a good understanding of the traffic issues in and around Kirkham and the immediate local area of the site having reviewed highway capacity and safety as a result of a number of recent development applications in the area. As such I expressed initial concerns given the scale of the proposed development and the layout of the initial access proposals submitted with the Transport Assessment. It was LCC's view that a significant proportion of vehicles traversing the network in the location of the proposed site access did so in excess of the signed 30mph limit. Therefore, in such circumstances where there is additional impact on the network

it will be expected that a developer will be required where necessary to provide appropriate measures to mitigate the impact of their proposal to deliver an acceptable solution.

These final comments consider all the highways and transport information provided with the application documentation; this information includes a Transport Assessment (TA) and a Travel Plan (TP) both produced by SKTP the developer's Transport Consultant. These comments also consider subsequent updated/further information in regard to the TA (traffic figures and speed survey information) a Technical Note (dated 10<sup>th</sup> December, provided in response to LCC's initial consultation comments of 29<sup>th</sup> July, 2015) and a further Technical Note (dated 17<sup>th</sup> February 2016). A revised site access layout (Scheme Drawing SK21542\_002 Rev E and a proposed 'Cycle Lane Provision Scheme' drawing (including traffic calming and Gateway measures, both sent to LCC on 9<sup>th</sup> May 2016).

In addition to the above, substantial further information relating to road safety was considered which was provided by a local resident (a retired Police Officer) as well as LCC's own analysis, site observations and surveys.

### **Development Proposal**

The proposal is an Outline Planning Application for the erection of 170 No. residential units and associated works, with access off B5192 Dowbridge, Kirkham.

LCC have provided considerable feedback to the developers Transport Consultant on this application site throughout the iterative planning process. I have reviewed the Transport Assessment (TA) and associated documentation and while there were a number of inconsistencies and anomalies identified in some areas, for example: Traffic data, Network description and Accessibility assessment, I consider the TA and additional information provided by the applicant a reasonable basis upon which to assess the highway influence and impacts of this proposal. This pragmatic and balanced approach relies on officer experience, understanding and judgement of the significance of the anomalies (and where necessary collecting and assessing further data for comparison) in coming to a conclusion that can be scrutinised in an appeal situation.

### **Existing Site Use**

This residential application is proposed on the site of the existing Brook Farm, Dowbridge. The existing site contains a large number of farm buildings. I would note that the extant permission on the site has the potential to generate traffic movements on the local highway network and given the nature of the permitted land use a number of the generated movements would be commercial/heavy goods vehicles. This has been taken into consideration by the highway authority.

### **Access Strategy**

It is proposed that vehicular access to the proposed 170 residential dwellings will be from a single junction off B5192 Dowbridge. The proposed access is to be provided in the location of the existing Brook Farm access and the original proposed layout was shown in Appendix G of the TA.

Some 40m to the west of the proposed site access is the Oxford Road residential access and some 40m to the east of the proposed site access is New Hey Lane.

The proposed access submitted with the TA gave rise to a number of concerns. These required further consideration by the applicant in order to deliver an acceptable access arrangement, one that could be agreed and which would satisfactorily address issues raised by LCC. I highlighted the following initial concerns to the developers Transport Consultant:

- I had concerns that there was only one access into a development of this scale (170 dwellings). The masterplan did not indicate any provision for emergency access. I requested further consideration for emergency access provision; The applicant has now confirmed that a separate emergency access provision from New Hey Lane is to be provided. This connection will also be made suitable for cycle access to New Hey Lane.
- I expressed concerns that observed speeds (85<sup>th</sup> percentile) in the vicinity of the proposed access junction were likely to be higher than the signed speed limit. I considered a review of observed speeds in the vicinity of the proposed site was warranted and therefore a speed survey would be required! Subsequently SKTP carried out a 24 hour speed survey. Given the importance of this issue and my concerns LCC also carried out our own surveys over a full week.

The speed surveys established the necessary visibility splays that would need to be achieved but also further reinforced my view that an appropriate traffic calming/gateway scheme, to be delivered as part of the site access s278 highway works, would be required as a minimum to achieve an acceptable access. The further information collated led to the development of the proposed site access layout and associated highway improvement works. This was an iterative process and the principles of the agreed scheme are set out under the heading s278 works on page 7 below.

#### Pedestrian Routing - Potential Issue

LCC are aware of concern raised by a resident (No. 8 Friary Close, off Oxford Drive). The resident is concerned that there is potential for a short-cut through their garden, given the Dow Brook is culverted in this location (making crossing in this location much easier). Any individuals crossing the Brook in this location from the proposed residential site could then easily get to Friary Lane and onto Oxford Drive from where there is a pedestrian link through to Dowbridge Road. LCC Highways are of the opinion that the resident has raised a very valid concern and we in turn have highlighted this potential issue with the applicant's representative.

It is clearly in the applicants gift to ensure the detailed design of the internal site layout (to be the subject of any reserved matters application) minimise any potential for this unacceptable routing to take place. In raising this matter again in these statutory consultation comments at this outline stage, it is hoped that local planning authority and the developer will work together to ensure this concern is suitably addressed through the detailed design layout.

#### Sustainable Transport

As part of the reforms of planning policy, the Department of Community and Local Government published the National Planning Policy Framework (NPPF), DCLG 2012. In terms of Transport, the NPPF sets out the principles that 'plans and decisions should ensure developments that generate significant movement are located where the need to travel will be minimised and the use of sustainable transport modes can be maximised.

Should the LPA be minded to approve this application, it would be appropriate to seek planning obligation contributions from this development to support improvements to the local network and sustainable transport links. This funding will be used to implement changes to limit the negative impact of this large development on the existing network.

### Pedestrian and Cycling Measures

It is clear there will need to be good provision of pedestrian/cycle routes through the site to the site access and also the existing bus stops. I requested that the developer give further consideration to the delivery of measures to support improvements for pedestrians and cyclists to improve connectivity to amenities in Kirkham and Wesham and support for wider connectivity improvements, for example to and within Kirkham Rail Station and to the main town centre and beyond (e.g. existing employment areas, education establishments and retail). These improvements to pedestrian/cycle links will help promote sustainable journeys.

This proposal creates an opportunity to improve connectivity for pedestrian/cycle movements by connecting route 62 of the NCN from New Hey Lane on to Carr Lane and the northern loop route. Delivery of a shared pedestrian/cycle route (3.5m width) through the site from the access track off New Hey Lane in the east through to the northwest of the site and beyond (to the church and primary school and on via FP5 through the park to Morrisons and the Railway station) would significantly improve connectivity and also help to address the single access issue for this 170 dwellings site (i.e. addressing emergency access requirements). This development can support delivery of an initial section of this route.

### Public Transport - Bus

I consider the existing bus stop for eastbound services, immediately adjacent to the proposed site access may need to be re-located slightly to the west. The optimum location for the bus stop should be considered and implemented as part of the s278 site access/traffic calming highway improvement scheme. In addition, both the eastbound and westbound bus stops located closest to the proposed site access should be upgraded to Quality Bus Standard as appropriate. This work should also to be delivered through a s278 agreement.

Any proposed PT improvements should be delivered early in the development build out to support PT from the earliest opportunity. The current bus services in the immediate vicinity of the site have been reviewed by LCC with consideration for the latest position with respect to funding of subsidised services. The latest position (as 8<sup>th</sup> March 2016) is that Service 61 will continue to operate, however, Service 75 will be revised and therefore only partly retained.

### Public Transport - Rail

Improvements to facilities for user of the Rail Station should be fully explored including appropriate funding to support an approach which seeks to be in line with NPPF and maximize use of sustainable modes by residents of the proposed development site.

I consider the developer should ensure that every opportunity is taken to enhance pedestrian/cycle routes to the Rail Station. The need for level access at the station has been highlighted as an issue. In an agreement reached on a recently approved residential development at Mowbreck Lane, the LPA made request for a contribution towards improvement measures of £1000 per plot for.

### Sustainable Measures to be Funded by the Developer

Section 106 funding contribution towards a range of sustainable transport measures (pedestrian/cycle/safety improvements) has been considered and a balanced approach taken with consideration for the final agreed s278 improvement works. The agreed s106 funding measures are set out under the heading 'Planning Obligations (s106 Planning Contributions)' below. The balanced approach considers the latest position in regard to PT services and road safety. The necessary package of measures s106 and s278 includes the following:

- Improved linkages between the site and Kirkham Rail Station, the main town centre and existing employment areas, education establishments and retail;
- Funding for pedestrian cycle improvements at the Rail Station
- Travel Plan Support
- Funding of further speed review and if shown to be necessary additional speed reduction measures (SPID signing).

#### Personal Injury Accident (PIA) Data

Personal Injury Accident data for the most recently available 5 year period was presented in the TA. In summary, the TA concluded that there were no safety issues on the local highway network that would be exacerbated by the proposal.

I have reviewed the latest accident data and would conclude that the PIA data does not suggest any particular accident pattern that would be a cause for concern. However, I made it clear to the developers Transport Consultant that I had reason to believe, following a number of site visits, that a significant proportion of vehicles traversing the network in the location of the proposed site access did so in excess of the signed 30mph limit.

In my assessment I have also taken into consideration further information passed to LCC which included: additional local information in regard to damage only collisions; vehicle speeds and other relevant local factors. Therefore, given the additional impact on the network expected from this development, I requested that the applicant develop their site access/highway improvement scheme to provide appropriate measures to address observed vehicle speeds and safety issues raised that would help mitigate the impact of their proposal and which would deliver an acceptable access solution.

SKTP have carried out a 24hr speed survey which showed average speeds were 33mph E/B and 32mph W/B. In addition, LCC have also undertaken further week long surveys to gain a better understanding of vehicle speeds in the vicinity of the proposed site access. The surveys were used to better inform development of a necessary site access/highway improvement/traffic calming/gateway scheme and in particular potential measures both east and west of the proposed site access to promote a reduction in vehicle speeds.

With consideration for all the information that should be taken into account in assessing the acceptability of the site access/highway improvement scheme, including current design standards and local & national policy, I consider the scheme shown in Plan (Scheme Drawing SK21542\_002 Rev E, (passed to LCC on 9<sup>th</sup> May 2016) and agreed 'in principle' subject to detailed design provides an acceptable access layout to address issues identified. The access scheme is reinforced by the wider improvement scheme set out in Plan SK21542\_007 Rev A (also passed to LCC on 9<sup>th</sup> May 2016 and agreed 'in principle' subject to detailed design) and A white lining marking scheme as indicated in LCC email dated 1 March 2016 will be provided at the A583/Dowbridge Junction.

The exact location of the eastbound bus stop and all associated considerations (i.e. whether the position of the bus stop in relation to the proposed refuge island will allow/will not allow traffic to pass a waiting bus) should be considered/integrated into the overall detailed design.

I am satisfied that there is a solution that can be delivered under a s278 agreement and the detail can be agreed at detail design stage. I am satisfied that the bus stop can be located in a position that will not impede access to private driveways etc.

Therefore, I consider at this stage it is sufficient that it is agreed that the bus stop (and the quality bus standard (QBS) raised kerb) will be located as appropriate when considered as part of the detailed design (s278 works) for the overall site access/highway improvement scheme. The agreed plan has been amended with appropriate wording to reflect this position.

An independent Stage 1 Road Safety Audit (RSA) was undertaken for the proposed access scheme agreed 'in principle'. A number of changes were made to the site access layout scheme in line with the recommendation of the Stage 1 RSA. I would note that the scheme now 'agreed in principle' may be subject to change as part of detailed design under a s278 agreement and will pick up a number of further detailed design matters raised in the Stage 1 RSA.

#### Travel Plan

A Framework Travel Plan (FTP) was submitted with the application documentation. LCC's Travel Plan Team provided comments to the developers transport consultant that identified a small number of omissions. A revised FTP was provided to LCC dated 20<sup>th</sup> November 2015 that addressed the issues raised.

For a development of this size we request a contribution of £12,000 to enable Lancashire County Councils Travel Planning Team to provide a range of Travel Plan services.

#### Funding to Support the Measures and Targets set within the Travel Plan

If Fylde Council were minded to approve this application, a commitment from the developer would be required to ensure appropriate funding is available to support the measures and targets of the Travel Plan. This funding would only be required if Travel Plan targets are not achieved (and is to be made available to the developers appointed travel plan coordinator and not passed to the LPA or the LHA).

Note: the funding must have the potential to deliver a real change to more sustainable modes. Such a change could be delivered through funding towards a bike (and safety equipment) for each household and a month's travel on public transport to encourage modal shift. The level offered must be adequate to deliver the measures necessary to support the targets within the Travel Plan. LCC consider funding of £180 per dwelling is appropriate for this site and to be retained by the developers appointed travel plan coordinator (and not LCC or Wyre) for 5 years from first occupation. This has been agreed by the applicant.

#### **Internal Site Layout, Parking Standards/Parking Provision and SUDS**

In respect of the current outline application, while acknowledging that internal layout matters will be picked up at the reserved matters stage, I would make the following observations based on the Outline Masterplan:

- The internal site layout should support the principles of 'Manual for Streets' and LCC's Creating Civilised Streets. There are a number of concerns with the layout as currently shown in the Masterplan;
- The Masterplan layout must include the emergency access proposal off New Hey Lane;
- The Layout will need further consideration by the applicant in regard to initial access road width, frontage access, parking control etc.;
- there will be a need for 1.8m service strips on access roads;

- Adequate parking provision, considering both visibility splays and manoeuvrability from all proposed parking locations will be required from a planning perspective (considering highway safety and impact on the highway);
- If the developer wishes to see the street(s) adopted then adequate parking provision, considering both visibility splays and manoeuvrability from all proposed parking locations will be required to LCC adoptable standards;
- high quality pedestrian linkages should be provided from the residential areas to the perimeter footways;
- all shared footway/cycleways should be delivered as a 3.5m wide facility;
- The Masterplan and site layout indicates the use of trees/planting both adjacent to and within streets that may be proposed for future highway adoption by the applicant. I would note that the LHA would not wish to take on significant maintenance issues created by the proposals as shown (in terms of root systems that may damage the carriageway and safety issues created by falling leaves). The provision of any trees, shrubs or plants must be agreed at the detailed design stage for their suitability, type and location. Planting will not be permitted where this would reduce visibility splays;
- In line with recent government policy I would expect the development to provide electric vehicle charging infrastructure at appropriate locations;
- There is a need to ensure appropriate access for servicing, delivery and waste collection to all properties.; the proposals should ensure that the layout is suitable for adoption at a later stage - should this be the intention of the applicant;
- Parking to the appropriate Fylde standards is expected - Parking Standards were set out in the emerging local plan which LCC consider reasonable, however, I would recommend seeking clarification from the LPA on the standards to be applied.
- I would ask the applicant to note at this stage the following in regard to driveway and garage dimensions; all integral garages must have internal dimensions of 3m x 6m or they will not be considered by LCC as part of the parking provision (refer also to bullet points above in relation to planning matters (highway safety / impact) and also with consideration for potential future highway adoption under a section 38 agreement with Lancashire County Council.
- LCC Highway Development Control section consider where garages are smaller than the recommended minimum internal dimension of 6m x 3m they should not be counted as a parking space and the applicant should provide an additional parking space for each garage affected;

#### Potential Pedestrian Routing Issue

LCC are aware of concern raised by a resident (No. 8 Friary Close, off Oxford Drive). The resident is concerned that there is potential for a short-cut through their garden, given the Dow Brook is culverted in this location.

- It is hoped the local planning authority and the developer will work together to ensure this concern is suitably addressed through any future detailed design layout.

#### Sustainable Urban Drainage Systems (SuDs)

LCC are now the Lead Local Flood Authority (LLFA), as such I would refer to the LCC Flood Risk Assessment Team detailed comments which, as a statutory consultee, are provided under a separate response;

- This application should fully consider the requirements that may be expected in order to support and deliver SUDs drainage (where deemed appropriate);
- I would expect the proposed drainage system to be designed to provide adequate capacity following current best practice and required standards that may allow consideration of adoption if deemed appropriate by the relevant authority. I would expect these drainage

- matters to be a condition of any approval if Fylde BC were minded to approve this application;
- In general, LCC will seek to limit the use of culverts where alternative sustainable solutions can be found.

### **S278 Works**

Should the Local Planning Authority be minded to approve this application a Section 278 Agreement for off-site highway improvements would be expected between the developer and the local highway authority, which for this proposal includes the site access/highway improvement scheme, a highway improvement scheme at A583 Kirkham Bypass/B5192 Dowbridge (white lining/markings renewal/update scheme) and a wider improvement scheme 'Proposed Cycle Lane Provision' scheme.

The site access/highway improvement scheme, agreed 'in principle' at this stage, will be subject to detailed design. The agreed scheme is shown in the revised Layout Drawing (Scheme Drawing SK21542\_002 Rev E, passed to LCC on 9th May 2016).

The proposed s278 works are expected to include the following measures:

- Site access junction;
- traffic calming/gateway measures - highway improvement scheme;
- Public Transport facilities to quality bus standard;
- With regard to the site access layout, the location of the eastbound bus stop will require further consideration as set out previously under the headings 'Public Transport' and also 'Personal Injury Accident (PIA) Data' above;
- a suitable lighting scheme to be provided at the access;
- The access junction will require to be delivered to adoptable standards with appropriate width to provide 1.8m service strips etc.;
- review of TRO's necessary to support the access proposals and potential Gateway measures etc. (all works to be carried out will form part of the access/off-site highway works under s278 agreement;
- The agreed layout plan confirms that the site access road gradients are to be constructed to the appropriate LCC adoptable standards;
- The revised Scheme Drawing SK21542\_002 Rev D, also confirms that the s278 works will include traffic calming works to the west of Oxford Road (Oxford Drive - Glebe Lane) to be included as part of detailed scheme design to LCC's specification.

The access scheme is reinforced by the wider improvement scheme set out in Plan SK21542\_007 Rev A (also passed to LCC on 9<sup>th</sup> May 2016 and agreed 'in principle' subject to detailed design)

- Advisory cycle lanes
- Gateway Measures
- Pedestrian refuge island

(Note: it has been agreed that the trigger point for the works shown in Plan SK21542\_007 Rev A is to be 25 dwellings or 18 months from start of Construction, whichever is sooner).

In addition the developer will deliver a white lining/markings renewal/update scheme at /A583 Kirkham Bypass/B5192 Dowbridge

- renewal of existing road markings;
- review and update to include new give way triangle and slow markings and additional hatching to at eastern give way.



The Trigger points for s278 works will be before commencement of development unless otherwise agreed with LCC and the LPA.

### **Planning Obligations (s106 Planning Contributions)**

It is appropriate to seek planning obligation contributions from this development to support improvements to the local network and sustainable transport links. This funding will be used to implement changes to improve routes to amenities; employment, retail and recreation from this development to the wider network.

Section 106 funding contribution towards a range of sustainable transport measures (pedestrian/cycle/safety improvements) has been considered and a balanced approach taken with consideration for the final agreed s278 improvement works.

The planning contributions requested and agreed are detailed below:

- (i) £15,000 Funding for pedestrian cycle improvements at the Rail Station

(It is suggested that the trigger point for the payment of this contributions should be on occupation of the 80<sup>th</sup> dwelling.)

- (ii) £10,000, Funding for further speed measurement survey (prior to occupation of the 51<sup>st</sup> Dwelling) and funding for further speed reduction measures (if necessary)

The Applicant/Developer will be required to fund a traffic speed review in the vicinity of the site access on the occupation of the 50<sup>th</sup> dwelling. Should 85<sup>th</sup> percentile speeds be greater than 30mph in either direction then further s106 funding will be triggered in order for the developer to deliver additional measures (in particular SPID signing).

- (iii) £12,000, Travel Plan Support - LCC request a sum appropriate for a development of this scale and in line with LCC's Planning Obligations Policy Paper, to enable Lancashire County Councils Travel Planning Team to provide a range of Travel Plan services. (Trigger - prior to the occupation of the 1<sup>st</sup> dwelling).

In addition, if Fylde Council were minded to approve this application, a commitment from the developer would be required to ensure appropriate funding is available to support the measures and targets of the Travel Plan asset out above on page 6, under the heading Travel Plan) This funding would only be required if Travel Plan targets are not achieved (and is to be made available to the developers appointed travel plan coordinator and not passed to the LPA or the LHA).

### **Summary and Recommendation**

This development will result in increased flows on the existing transport network in and around the development site. LCC Highways Development Control expressed our initial concerns in respect of this application given the scale of the proposed development, the initial access proposal and observed traffic speeds in this location. However, LCC Highways operate a 'one team' approach and will always endeavour, where possible, to engage with developers and their transport representatives to give them an opportunity to address our concerns.

The developers Transport Consultant (SCKTP) has provided further information, including mitigation measures, since the submission of the original Transport Assessment. LCC have

also carried out our own further analysis to fully understand the highway influence and impacts of this proposal before reaching a conclusion.

LCC as local highway authority consider that, if all measures as detailed in the sections titled 'Planning Obligations' and 'S278 Works' above were provided then the residual cumulative impacts of the development would not be severe or compromise overall safety.

**With consideration for all the information now provided, LCC would have no objection to the proposed development providing that appropriate funding (s106) for sustainable measures is secured and that all s278 measures as agreed and detailed above are delivered by the developer in line with agreed trigger points. It is essential that suitable conditions are put in place to ensure these necessary measures are delivered.**

I attach below a list of suggested conditions that may be appropriate should the LPA be minded to grant approval.

***Suggested Conditions:***

- 1 No part of the development hereby approved shall commence until a scheme for the construction the site access and the off-site works of highway improvement have been submitted to, and approved by the Local Planning Authority in consultation with the Highway Authority.

Note:

(i) Delivering these works will require a review, consultation and implementation of new/or changes to TROs; the full cost for these to be funded by the developer.

(ii) The position of the eastbound bus stop will be subject to detailed design.

(iii) As part of the detailed design, the issues identified in the Stage 1 Safety Audits for the off-site highway works will be addressed through the S278.

(iv) The site access gradients will be constructed to the appropriate LCC adoptable standards.

*Reason: In order to satisfy the Local Planning Authority and the Highway Authority that the final details of the highway scheme/works are acceptable before work commences on site. Also, in order to provide safe access to the site for all users (motorised and non-motorised).*

- 2 Prior to the first occupation of any part of the development hereby approved, the access and off-site highway works set out in condition 1 should be constructed in accordance with the details approved, unless otherwise agreed in writing with the Local Planning Authority.

*Reason: In order to satisfy the Local Planning Authority and the Highway Authority that the development will provide a safe access to the site and ensure that users of the development have appropriate access to sustainable transport options*

- 3 Prior to occupation of the 51<sup>st</sup> dwelling the Applicant/Developer will be required to carry out a traffic speed review in the vicinity of the site access. Should the review indicate that the 85<sup>th</sup> percentile speeds are greater than 30mph (in either direction) this will trigger

further s106 funding in order for the developer to deliver additional measures (in particular SPID signing).

*Reason: In order to satisfy the Local Planning Authority and the Highway Authority that the development will provide a safe access to the site.*

- 4 The Framework Travel Plan as agreed must be implemented in full in accordance with the timetable within it unless otherwise agreed in writing with the Local Planning Authority. All elements shall continue to be implemented at all times thereafter for as long as any part of the development is occupied or used/for a minimum of at least 5 years.

*Reason: To ensure that the development provides sustainable transport options.*

- 5 No development shall take place until a Construction Method Statement has been submitted to, and approved in writing, by the Local Planning Authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors;
- ii) loading and unloading of plant and materials;
- iii) storage of plant and materials used in constructing the development;
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate;
- v) wheel washing facilities;
- vi) a management plan to control the emission of dust and dirt during construction identifying suitable mitigation measures;
- vii) a scheme for recycling/disposing of waste resulting from construction work (there shall be no burning on site);
- viii) a Management Plan to identify potential ground and water contaminants; details for their storage and how water courses will be protected against spillage incidents and pollution during the course of construction;
- ix) a scheme to control noise during the construction phase, and
- x) the routing of construction vehicles and deliveries to site.

*Reason: To maintain the operation and safety of the local highway network during site preparation and construction.*

- 6 There shall not at any time in connection with the development hereby permitted be planted hedges, trees or shrubs over 1m above the road level within any visibility splay required to maintain safe operation for all users.

*Reason: To ensure adequate visibility splays are maintained at all time.*

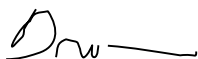
- 7 The development hereby approved shall not be commenced until a scheme for the provision of surface and foul water drainage works, with full consideration for sustainable drainage principles, has been submitted to and approved by the Local Planning Authority. The means of drainage shall be implemented in accordance with the approved scheme, prior to first occupation of the development hereby approved.

*Reason: To reduce the increased risk of flooding by ensuring provision of a satisfactory means of surface and foul water disposal.*

**The following informative notes should be added to any approval granted:**

- a. The grant of planning permission does not entitle a developer to obstruct a right of way and any proposed stopping-up or diversion of a right of way should be the subject of an Order under the appropriate Act.
- b. The grant of planning permission will require the applicant to enter into an appropriate Legal Agreement with the County Council as Highway Authority. The Highway Authority hereby reserves the right to provide the highway works within the highway associated with this proposal. Provision of the highway works includes design, procurement of the work by contract and supervision of the works. The applicant should be advised to contact the Environment Director at County Hall, Preston PR1 0LD, in the first instance, to ascertain the details of such an agreement and the information to be provided.
- c. Traffic Regulation Orders, diversions of Public Rights of Way, Stopping Up of existing highway, changes to public transport scheduling/routing and other activities require separate statutory consultation processes beyond the planning application process. The applicant will be obliged to meet all the costs associated with these of works and ensure that any works which rely upon them do not commence until all legal processes have been satisfactorily completed.

Yours Faithfully



David Watson  
Principal Engineer  
Highways Development Control